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## REPORT TO PLAN COMMISSION

**FROM:** Bruce A. Knight, FAICP, Planning and Development Director

**DATE:** January 10, 2014

**SUBJECT: PL13--0071 MAJOR AMENDMENT TO THE PRELIMINARY/FINAL  
PLANNED DEVELOPMENT FOR  
CAMPUS CENTER (PARKING LOT J)  
(524 - 526 E. GREEN AND 601 S. SIXTH ST.)**

**A. Introduction:** The applicant requests a major amendment to the approved Preliminary/Final Planned Development for Campus Center (Parking Lot J) at 524-526 E. Green Street and 601 S. Sixth Street. The amendment would reduce the development to six stories and make other architectural changes.

**B. Recommended Action:** Staff recommends the Plan Commission forward the proposed Major Amendment to the Preliminary/Final Planned Development to the City Council with a recommendation for approval after finding that the application meets the requirements and findings of Section 37-308, Final Major Planned Development Plan Approval.

**C. Prior Council Action:**

- At a special Regular Meeting on April 9, 2013, City Council adopted CB2013-056 and CB2013-057, approving the Preliminary/Final Planned Development Plan for Campus Center and the Development Agreement between the City of Champaign and Campus Center, LLC, for the sale and redevelopment of Parking Lot J.
- At its November 19, 2013 meeting, City Council adopted CB2013-206, approving the Campus Center Subdivision and vacating the east-west public alley between Green and Healey Streets and Fifth and Sixth Streets.
- At the December 10, 2013, Study Session, City Council supported project changes on a conceptual level.

**D. Summary:**

- In April 2013, the Development Agreement and Planned Development were approved by Council to construct two 12-story buildings and a 5-story parking garage on Parking Lot J.
- The developer finalized the construction drawings and received bids on constructions.
- Due to a variety of factors, the received bids were \$19 million more than budgeted for the project.
- The developer has revised the project design to ensure the project can be built with a reasonable budget.
- The developer proposes to reduce the heights of the buildings to 6 stories, remove the cantilever over the single-story building where Flat Top Grill and Penn Station are located,

remove the bridge over the Boneyard connecting the two buildings, reduce the size of the parking garage, and add two levels of residential units over the parking garage.

- The resulting proposed project would contain 90 residential units, 95 hotel rooms, and approximately 12,000 square feet of retail space.
- City Council supported the revised project in concept at its December 10, 2013, Study Session meeting. Minutes from that meeting can be reviewed here: <http://archive.ci.champaign.il.us/archive/dsweb/Get/Document-13041/2013-12-10%20Regular%20Study%20Session%20Minutes.pdf>.
- The approved Planned Development granted flexibility from some of the Zoning Ordinance requirements through several waivers: a reduction in the residential parking requirement; a reduction in the open space requirement; and slight modifications to the Campus Commercial Overlay to accommodate the parking garage, Boneyard Greenway, the hotel design, and enhanced facade design.
- The amendment includes many the same waivers, except that some of the specifics of the waivers have changed, and that the waiver for the reduction in open space is no longer needed due to an approved change to the Zoning Ordinance to eliminate the provision of open space in the CB, Central Business, Zoning District.
- Conditions are proposed to address parking garage lighting, public access easements, the bicycle repair station and bicycle parking, enhanced courtyard design, and landscape maintenance. These conditions have been revised to reflect additional design of the Boneyard Greenway area that has been completed.
- The revised project will also require City Council approval of amendments to the Development Agreement.

## **E. Background:**

### **1. Site Information.**

- a. Applicant: Campus Center, LLC (Members: Jeffrey R. Hartman, Christopher M. Hartman, Andrew J. Hartman, Patrick W. Hartman, and John S. Kunkel)
- b. Property Owners: City of Champaign, with Development Agreement and contract for sale to JSM Development pending
- c. Area: 1.2 acres
- d. Location: Southwest corner of Healey Street and Sixth Street, and including frontage on Green Street.
- e. Common addresses: 524 – 526 E. Green Street and 601 S. Sixth Street

## 2. Surrounding Property Characteristics.

	Zoning	Land Use	Comprehensive Plan
Subject Property	<b>CB, Central Business, with Campus Commercial Overlay</b>	<b>City-Owned surface parking lot</b>	<b>Campustown</b>
North	MF3, Multifamily High Density/Limited Business	Multifamily residential	University Neighborhood
East	CB, Central Business, with Campus Commercial Overlay	Retail, office, University-owned surface parking lot	Campustown
South	CB, Central Business, with Campus Commercial Overlay	Retail and office	Campustown
West	CB, Central Business, with Campus Commercial Overlay	Retail and Multifamily residential	Campustown

**3. Overview.** The approved Planned Development included two twelve-story mixed use buildings connected by a glass bridge, and a five-story parking garage. The design had the ground floor occupied by retail space, with four stories of hotel rooms above the retail space along Green Street and cantilevered over the building at 607 S. Sixth Street, which houses the Penn Station and Flat Top Grill restaurants. Above the remainder of the retail base along Sixth Street, and above the hotel units along Green Street, were between seven and eleven stories of multifamily residential units. The approved project contained 108 hotel rooms, 297 apartments that were either studios, one-bedrooms, or two-bedrooms, and approximately 21,000 square feet of retail space. The parking garage was planned with 131 public parking spaces and 228 parking spaces for the project itself.

The main changes proposed to the project as this Major Amendment are a reduction in the heights of the buildings from 12 stories to 6 stories, removing the bridges connecting the buildings and the cantilevered portion of the building over the existing building along Sixth Street currently containing Flat Top Grill and Penn Station, and replacing two levels of private parking within the garage with two levels of residential units. Most of the difference in building height is borne in the residential part of the project, which is decreased by two-thirds, and by the retail, which is almost halved. The amendment proposes 95 hotel rooms, 88 residential units, and approximately 12,000 square feet of retail space. Attachment B provides additional details on the project and the application.

As the revised project does not include the cantilevering component, JSM has indicated that a potential future redevelopment of the Flat Top/Penn Station property could occur once the leases for the current tenants have expired. Redevelopment could include an expansion for the proposed hotel of approximately 45 rooms over ground floor retail. If the redevelopment of the Flat Top/Penn Station site included connecting with the proposed buildings on Parking Lot J, this Planned Development would require a future Major Amendment to incorporate that redevelopment.

**4. Preliminary/Final Planned Development.** A Planned Development offers an alternative method for developing land by allowing flexibility in certain zoning standards. The approved Planned Development was granted flexibility from some of the requirements pertaining to the Campus Commercial Overlay, parking requirements, and open space requirements. This amendment requests similar waivers. These waivers are discussed in the section below.

Section 37-306 of the Zoning Ordinance requires that the Plan Commission must find that each of the following criteria are met before it can recommend approval of the Planned Development.

- a. The proposal advances the purpose statements of this Article;
- b. The proposal meets the minimum development requirements of this Article;
- c. The proposal adequately and appropriately incorporates the design guidelines in Table VI-D that are appropriate to the type of development being proposed.
- d. The proposed zoning is consistent with the Comprehensive Plan and is in the best interest of the public.
- e. Any proposed waivers or regulations or requirements of this Chapter meet the requirements of Section 37-327 of this Chapter, and any proposed waivers of Chapter 31 of this Code meet the requirements of Section 37-328 of this Chapter.

A detailed analysis of these criteria appears in Attachment C. In summary, the amended proposal advances the purpose statements of the Planned Development Article by providing infill development that provides additional amenities not required under conventional zoning by providing public parking, and adding a new land use (a hotel) to Campustown. The proposal also meets the minimum requirements, incorporates applicable design guidelines, is consistent with the Comprehensive Plan and is in the best interest of the public. The proposed waivers of the Zoning Ordinance meet the requirements of Section 37-327, and there are no requested waivers of Chapter 31.

**5. Waivers.** Seven waivers from the Zoning Ordinance are requested and supported by Staff:

a. *Sec. 37-125.1(c)(4). Building facades shall have a minimum width of 90% of lot frontage.* Due to the garage entrances on Healey Street and the Boneyard Greenway connection to Sixth Street, those elevations do not meet the 90% minimum and driveway entrances exceed 22 feet. *Justification:* The public parking provided and the continuation of the Boneyard Greenway, the multi-use trail that begins at Second Street, provides important benefits to the community and necessitates an adjustment to this requirement.

b. *Sec. 37-125.1(c)(5). Five foot setback of all floors above the third floor.* The Green Street façade sets back the requisite 5 feet at the second floor with the exception of two modules, each approximately 18' wide, that help define the linear character of the façade. These two modules maintain the grade level façade plane over the full height of the building. The grade level façade plane varies between 3 feet and 4 feet from the property line. One other exception to the set back is the main stair at the east end of the Green Street façade. Given the need to maintain a continuous stair enclosure over the full height of the building, no upper level setback is provided.

Due to the minimum site area necessary to accommodate a parking garage, the portion of the building on the northeast quadrant of the site has an atypically narrow footprint, limiting the

ability to achieve a full 5 foot setback of upper levels from the lower building façade. Thus, the Sixth Street façade sets back 3'-4" beginning at the second floor with the exception of one module approximately 20' wide that help define the linear character of the façade and serves as a massing cue for the building entry location. This module maintains the grade level façade plane over the full height of the building. Beginning at the second floor line, the upper floors of the Healey Street façade set back approximately 3 feet from the lower building façade, but maintain a 6 foot setback from the property line. One other exception to the set back is the main stair at the west end of the Healey Street façade. Given the need to maintain a continuous stair enclosure over the full height of the building, no upper level setback is provided. Given the structural and functional restrictions of the parking deck, no additional setback is anticipated for upper levels of the parking deck's Healey Street façade.

*Justification:* The majority of the building facades meet the requirement. The exceptions help to provide more articulation in the building façade and therefore a more interesting and detailed elevation.

c. *Sec. 37-125.1(c)(6). Transparency at the ground floor.* The project does not comply with the 75% minimum transparency requirement in the following locations: the parking deck and building service space of the narrow Healey Street frontage for which this requirement would not be reasonably applicable, and the façade along Green Street, which provides for 74% transparency to provide a base to the building that is architecturally in scale with the remainder of the building. Stair towers are also excluded from the frontage calculation.

*Justification:* A required component of the project, the public parking needs to be easily accessed and viewed from the public right-of-way to encourage its use. In terms of the Green Street façade, the brick piers on the ground level are in scale with the remainder of the building, resulting in a minimal reduction in the level of transparency at the ground level. This reduction allows for better architectural design to provide a solid base to the rest of the building. Emergency egress stair towers are a reasonable exception to the transparency requirement and are provided only where required by building code.

d. *Sec. 37-125.1(c)(7). Transparency above the ground floor.* The project does not comply with the 30% minimum transparency requirement for the Healey Street residential portions of the building (at 24%) to remain consistent with the punched window motif of the architectural style while maximizing the amount of transparency. Additional non-compliant facades are those facing a street but not actually fronting on the street, such as the eastern façade of the Green Street building and the southern façade of the Sixth Street building. Excluding the elevations not fronting on a street is reasonable for this project given (i) there is an existing two story building between the referenced elevations and the street, largely screening the elevations from view; and (ii) the referenced elevations set back from the property line between 70 feet to 100 feet, mitigating their perceptual impact on the overall façade. Stair towers are also excluded from the area calculation.

*Justification:* A portion of the Healey Street façade is the side of the narrow side of the building that fronts along Sixth Street, providing a limited area to provide windows. The other portion of the Healey Street façade, over the parking structure, provides the maximum transparency while maintaining a consistent architectural design with the remainder of the project. The other elevations in question "face" the street and therefore trigger the transparency requirement, but their setbacks of at least 70 feet results in the inability for a pedestrian at the ground level to be negatively impacted by a lack of windows. In addition, the developer has future plans to redevelop one on the adjacent sites, which would screen most of these facades in the future.

e. *Sec. 37-125.1(c)(12). Parking deck screening.* The Healey Street facing façade of the parking deck will provide partial screening at guardrail height. This height is tall enough to screen vehicle headlights from projecting out of the deck. The balance of the wall will be open to provide the building code required ventilation necessary for an open parking structure.

*Justification:* The provision of public parking offsets the screening requirement, as the public parking will be more easily located. The proposed construction materials for the walls are a patterned precast concrete panel system similar to the one used for the City-owned public parking garage in downtown.

f. *Section 37-324. Common Open Space Requirements for Major Planned Developments.* This section requires that a parcel designated as common open space of at least 6,000 square feet is conveyed to a not-for-profit corporation, guaranteed by a restrictive covenant, or dedicated to the Champaign Park District. This requirement is partially met by continuing the Boneyard Greenway and providing public access to both the Greenway and the adjacent courtyard, however the width of the proposed Greenway (10 feet) does not meet the minimum dimension for open space of 18 feet. Other common areas are dispersed throughout the development, including an interior pool and fitness area and four courtyards on the second and fifth floors.

*Justification:* Providing ground level open space in an urban environment is contrary to the goals of providing dense development that continues the building wall. The requirement to dedicate open space is partially met by this project by continuing the Boneyard Greenway and providing public access to both the Greenway and the adjacent courtyard, however the width of the proposed Greenway (10 feet) does not meet the minimum dimension for open space of 18 feet and therefore cannot be fully counted in the calculation, despite its full functionality as open space.

g. *Sec. 37-359. Required Number of parking Spaces by Use (specifically Sec. 37-359.2 Residential and Related Uses).* A reduction of 26 parking spaces is requested for the project. The zoning requirement for parking is 71 spaces for the 88 residential units. Neither the retail nor the hotel uses have a parking requirement due to their location within the Central Business District. While the project will provide a total of 176 parking spaces, 131 of those spaces are required to be accessible to the public through the Development Agreement. Therefore, as a practical matter, a total of 202 parking spaces are “required” for the project (71 residential spaces + 131 public spaces), leading to a request for a waiver of 26 parking spaces.

*Justification:* The provision of 131 public parking spaces is ample justification to reduce the parking requirement by 26 spaces.

**6. Conditions of Approval.** The approved conditions of approval of the Preliminary/Final Planned Development have been revised to reflect a change in attachment labeling and design work that has already occurred. The revised conditions are shown here with strike-outs and underlined new language.

- a. The site shall be developed in general conformance with the attached Site Plans and Elevations.
- b. The Owner agrees that failure to meet the conditions of this Planned Development may be considered a violation of the Zoning Ordinance and subject to the fines provided for in Article II of Chapter 22 of Champaign’s Municipal Code.

- c. The Owner agrees to provide lighting in the parking garage that can be dimmed and directed to minimize light spillover onto adjacent properties.
- d. The Owner agrees to provide public access easements, approved by the City Engineer, over the continuation of the Boneyard Greenway and for the vehicular driveways accessing the parking garage consistent with the ~~Site Plan~~ Easement Summary in Attachment ~~D~~ B.
- e. The Owner agrees to relocate the bicycle repair station from its current location on the Boneyard Greenway to a location on the ground floor of the parking garage that is accessible to the public. If the repair station is not visible from the Greenway, the Owner agrees to provide directional signage to the repair station that is visible from the Greenway.
- f. The Owner agrees to provide a minimum of ten (10) bicycle parking spaces on the ground floor of the parking garage accessible to the public and in excess of the Zoning Ordinance requirement for bicycle parking for the development.
- g. ~~Prior to issuance of a building permit, the Owner agrees to provide a revised site plan for the Boneyard Greenway and courtyard to depict additional amenities than those depicted on the attached Site Plan. At a minimum, the revised site plan shall include either 1) one piece of artwork affixed to or 2) decorative brickwork on the north elevation of the ground floor of the building fronting on Green Street and at least one of either 1) a freestanding piece of artwork, 2) additional seating or 3) additional landscaping within the courtyard space adjacent to the Boneyard Greenway.~~ The Owner agrees to install and maintain these additional amenities provided within the on-site Boneyard Greenway and the adjacent courtyard.
- h. The Owner agrees to maintain the proposed landscaping in compliance with Section 37-573 of the Zoning Ordinance.

**7. Next Steps.** City Council will consider a revised Development Agreement concurrently with this Major Amendment to the Planned Development in February 2014. Construction can begin in spring of this year, with completion of the project slated for August 2015, which is consistent with the original completion date.

**F. Alternatives:**

- 1. Recommend City Council approve the Major Amendment to the Preliminary/Final Planned Development.
- 2. Recommend City Council deny the Major Amendment to the Preliminary/Final Planned Development.

## **G. Discussion of Alternatives:**

**Alternative 1** recommends approval of the Major Amendment to the Preliminary/Final Planned Development.

### **a. Advantages**

- Allows redevelopment of Parking Lot J consistent with the proposal selected as part of a Request for Proposals process.
- Encourages infill development and the construction of 131 public parking spaces.
- Brings a new type of land use (hotel) to Campustown.

### **b. Disadvantages**

- None.

**Alternative 2** does not recommend approval of the Major Amendment to the Preliminary/Final Planned Development.

### **a. Advantages**

- None.

### **b. Disadvantages**

- Would not allow redevelopment of Parking Lot J consistent with the proposal selected as part of a Request for Proposals process.
- Would not encourage infill development and the construction of 131 public parking spaces.
- Would not bring a hotel to Campustown.

**H. Community Input:** Notice of this Major Amendment to the Preliminary/Final Planned Development was published in the December 31, 2013, edition of the News-Gazette for a public hearing before the Plan Commission, and letters have been sent to all adjacent property owners encouraging public input. A sign advertising the Preliminary/Final Planned Development was placed on the property. Additional public input can also be provided at the Plan Commission public hearing.

**I. Budget Impact:** The developer will pay the City approximately \$3.94M to purchase the land per the provisions of the Development Agreement. City financial involvement following the sale would be to offset an expected shortfall in the revenue generated by the public parking component of the project during the early years of operation. The City also expects to undertake a street reconstruction project of Healey Street between Sixth and Fourth Streets. The applicant will share in the cost of a portion of that project.

**J. Staffing Impact:** Approximately twenty hours of staff time from Planning and Legal were spent in the review of the Planned Development application and preparation of report materials.

Prepared by:

Reviewed by:

Lorrie Pearson, AICP  
Land Development Manager

Bruce A. Knight, FAICP  
Planning and Development  
Director

Attachments: A: Location Map  
B: Planned Development Plan Certificate with application, elevations, site plan  
C: Planned Development Criteria for Approval

# PL13-0071 Planned Development Amendment Campus Center (Parking Lot J)



**Major Amendment to the Preliminary/Final  
Planned Development Plan  
Campus Center (Parking Lot J)  
524 - 526 E. Green Street and 601 S. Sixth Street**

**1. Location:**

**524-526 E. Green Street, Champaign, Illinois:**

**PIN:** 46-21-18-130-013, 46-21-18-130-014

**601 S. Sixth Street, Champaign, Illinois:**

**PIN:** 46-21-18-130-004, 46-21-18-130-005, 46-21-18-130-006

**2. Project Data (all numbers approximate):**

- Zoning: CB, Central Business, within the Campus Commercial Overlay District
- Proposed Use: Mixed Use: Retail, Hotel, Multi-family residential
- Number of buildings: 3 (parking garage and two buildings containing retail, hotel, and residential)
- Land area: 52,569 square feet (1.207 acres)
- Gross Square Footage of Buildings: 448,840 sf
- Net Square Footage of Buildings: 110,680 sf
- Lot coverage of building: 84%
- Total number of dwelling units: 88 DU, 95 hotel rooms
- Residential densities: 73 du/ac
- Floor Area Ratio (FAR) Maximum: 6.0
- Floor Area Ratio (FAR) Provided: 2.6
- Open Space Required: 6,000 sf of a single common area for a Planned Development
- Open Space Provided: 6,730 sf, divided among areas and including interior spaces (waiver requested as open space is dispersed, smaller than 30' in width, and in some cases interior open space)
- Parking calculations: 71 spaces required by Zoning + 131 public parking spaces required by Development Agreement = 202 spaces; 176 total spaces provided (waiver requested for 26 spaces)
- Building height: 80 ft.

**3. Existing Conditions:** City-owned surface parking lot and Boneyard Greenway

**4. Site Plan and Landscape Plan:** See attached.

**5. Floor Plans:** See attached.

**6. Elevation/Perspective Drawing:** See attached.

**7. Development Schedule:** Construction to begin: Spring 2014 / Completed: summer 2015

**8. List of Waivers:**

a. *Sec. 37-125.1(c)(4). Building facades shall have a minimum width of 90% of lot frontage.* Due to the garage entrances on Healey Street and the Boneyard Greenway connection to Sixth Street, those elevations do not meet the 90% minimum and driveway entrances exceed 22 feet.

b. *Sec. 37-125.1(c)(5). Five foot setback of all floors above the third floor.* The Green Street façade sets back the requisite 5 feet at the second floor with the exception of two modules, each approximately 18' wide, that help define the linear character of the façade. These two modules maintain the grade level façade plane over the full height of the building. The grade level façade plane varies between 3 feet and 4 feet from the property line. One other exception to the set back is the main stair at the east end of the Green Street façade. Given the need to maintain a continuous stair enclosure over the full height of the building, no upper level setback is provided.

Due to the minimum site area necessary to accommodate a parking garage, the portion of the building on the northeast quadrant of the site has an atypically narrow footprint, limiting the ability to achieve a full 5 foot setback of upper levels from the lower building façade. Thus, the Sixth Street façade sets back 3'-4" beginning at the second floor with the exception of one module approximately 20' wide that help define the linear character of the façade and serves as a massing cue for the building entry location. This module maintains the grade level façade plane over the full height of the building. Beginning at the second floor line, the upper floors of the Healey Street façade set back approximately 3 feet from the lower building façade, but maintain a 6 foot setback from the property line. One other exception to the set back is the main stair at the west end of the Healey Street façade. Given the need to maintain a continuous stair enclosure over the full height of the building, no upper level setback is provided. Given the structural and functional restrictions of the parking deck, no additional setback is anticipated for upper levels of the parking deck's Healey Street façade.

c. *Sec. 37-125.1(c)(6). Transparency at the ground floor.* Project does not comply with the 75% minimum transparency requirement in the following locations: the parking deck and building service space of the narrow Healey Street frontage for which this requirement would not be reasonably applicable, and the façade along Green Street, which provides for 74% transparency to provide a base to the building that is architecturally in scale with the remainder of the building. Stair towers are also excluded from the frontage calculation.

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the architectural style while maximizing the amount of transparency. Additional non-compliant facades are those facing a street but not actually fronting on the street, such as the eastern façade of the Green Street building and the southern façade of the Sixth Street building. Excluding the elevations not fronting on a street is reasonable for this project given (i) there is an existing two story building between the referenced elevations and the street, largely screening the elevations from view; and (ii) the referenced elevations set back from the property line between 70 feet to 100 feet, mitigating their perceptual impact on the overall façade. Stair towers are also excluded from the area calculation.

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f. *Section 37-324. Common Open Space Requirements for Major Planned Developments.* This section requires that a parcel designated as common open space of at least 6,000 square feet is conveyed to a not-for-profit corporation, guaranteed by a restrictive covenant, or dedicated to the Champaign Park District. This requirement is partially met by continuing the Boneyard Greenway and providing public access to both the Greenway and the adjacent courtyard, however the width of the proposed Greenway (10 feet) does not meet the minimum dimension for open space of 18 feet. Other common areas are dispersed throughout the development, including an interior pool and fitness area and four courtyards on the second and fifth floors.

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### **9. Conditions of Approval:**

1. The site shall be developed in substantial conformance with the attached Site Plans and Elevations.
2. The Owner agrees that failure to meet the conditions of this Planned Development may be considered a violation of the Zoning Ordinance and subject the Owner to the fines provided for in Article II of Chapter 22 of Champaign’s Municipal Code.

## **Attachment B**

3. The Owner agrees to provide lighting in the parking garage that can be dimmed, directed, or controlled with cutoff or screening assemblies to minimize light spillover onto adjacent properties.
4. The Owner agrees to provide public access easements, approved by the City Engineer, over the continuation of the Boneyard Greenway and for the vehicular driveways accessing the parking garage consistent with the Easement Summary in Attachment B.
5. The Owner agrees to relocate the bicycle repair station from its current location on the Boneyard Greenway to a location on the ground floor of the parking garage that is accessible to the public. If the repair station is not visible from the Greenway, the Owner agrees to provide directional signage to the repair station that is visible from the Greenway.
6. The Owner agrees to provide a minimum of ten (10) bicycle parking spaces on the ground floor of the parking garage accessible to the public and in excess of the Zoning Ordinance requirement for bicycle parking for the development.
7. The Owner agrees to install and maintain the amenities provided within the on-site Boneyard Greenway and the adjacent courtyard.
8. The Owner agrees to maintain the proposed landscaping in compliance with Section 37-573 of the Zoning Ordinance.



# PRELIMINARY PLANNED DEVELOPMENT APPLICATION

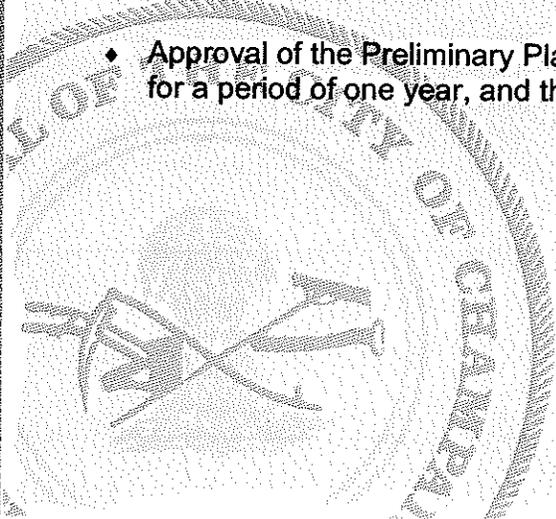
## Champaign Municipal Code Section 37-301.

### Sec. 37-301. Purpose.

The regulations for Planned Developments offer an alternative method for developing land. This alternative allows flexibility in applying certain zoning standards. Such flexibility requires a review process and development plan to safeguard health, safety, and welfare concerns. In exchange for flexibility, Planned Developments are required to provide amenities not otherwise required through traditional zoning techniques. These requirements are designed to offset the impact of changes in development standards allowed through these provisions such as increased densities, mixed land uses and reduced setbacks. The Planned Development designation acts as an overlay zone. With this approach, the overall intensity of a development is consistent with the underlying zoning district and Comprehensive Plan.

### Notice To Applicants

- ◆ Before an application is accepted, a conference with Current Planning staff is required.
- ◆ There will be no refund of any application fee for approvals not granted or withdrawn.
- ◆ No incomplete applications will be acted upon.
- ◆ Complete applications must be received not less than 30 days prior to the next regular meeting of the Plan Commission in order to be placed on the agenda for the meeting. Late submittals will not be acted upon without prior approval by the Planning Director.
- ◆ The applicant is advised to include all attachments required as a part of the application.
- ◆ Approval of the Preliminary Planned Development by the City Council shall be in effect for a period of one year, and thereafter is null and void.



City of Champaign  
Planning Department  
102 North Neil Street  
Champaign, IL 61820  
Phone: (217) 403-8800  
Fax: (217) 403-8810

# Preliminary Planned Development Application

Office Use:  
Date Received: \_\_\_\_\_  
Fee: \_\_\_\_\_  
Date Fee Paid: \_\_\_\_\_  
Case No: \_\_\_\_\_

Name of Development: Campus Center

Common Address: 524 E. Green St. and 601 S. Sixth St.

Parcel Identification Number: 46-21-18-130-004, 005, 006, 013, 014

Name of Developer: Campus Center, LLC

505 S. Fifth St.  
Street Address

Champaign IL 61820  
City State Zip

Phone:  
Work: 217-359-5828  
Mobile: \_\_\_\_\_  
Fax: 217-359-5839

E-mail: kunkel@jsmliving.com

Name of Local Agent:

Kunkel Scott John  
Last Middle First

505 S. Fifth St.  
Street Address

Champaign IL 61820  
City State Zip

Phone:  
Work: 217-359-5828  
Mobile: \_\_\_\_\_  
Fax: 217-359-5839

E-mail: kunkel@jsmliving.com

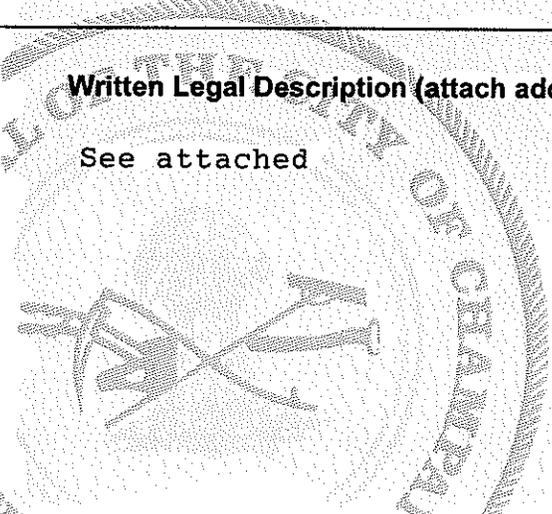
Written Legal Description (attach additional pages if necessary):

See attached

DEVELOPER INFORMATION

LOCAL AGENT

LEGAL DESCRIPTION



The Owner:  is/are individual(s).....(Go to 1. below)  
 a partnership..... LLC.....(Go to 2. below)  
 a corporation.....(Go to 3. below)  
 a trust.....(Go to 4. below)

**1. Names of Individual(s) - (as in property title):**

\_\_\_\_\_  
 Last Middle First  
 \_\_\_\_\_  
 Street Address  
 \_\_\_\_\_  
 City State Zip

*Use Section 1 of Owner Authorization Form*

**2. Name of Partnership LLC:**

\_\_\_\_\_  
 Campus Center, LLC  
 505 S. Fifth St.  
 \_\_\_\_\_  
 Street Address  
 \_\_\_\_\_  
 Champaign IL 61820  
 City State Zip

*Use Section 2 of Owner Authorization Form*

**3. Name of Corporation:**

\_\_\_\_\_  
 \_\_\_\_\_  
 Street Address  
 \_\_\_\_\_  
 City State Zip

*Use Section 3 of Owner Authorization Form*

**4. Name of Trust and Number:**

\_\_\_\_\_  
 Name Number  
 \_\_\_\_\_  
 Street Address  
 \_\_\_\_\_  
 City State Zip

*Use Section 4 of Owner Authorization Form*



Gross Acreage of Development: 1.207

Square Feet: 52,569

Zoning Classification: CB

Existing Land Use: City of Champaign municipal parking lot (surface parking)

Proposed Land Use(s): Mixed-use development with retail, hotel, multi-family residential, and structured parking.

Number of Buildings: 3 Number of Dwelling Units: 88

Percent of Lot Coverage: 84%

Floor Area Ratio: 2.60 Open Space Ratio: N/A

Residential Density (i.e. 5 dwelling units per acre): 73 D.U./Acre

Number of Parking Spaces:

Required: 202\* Provided: 176

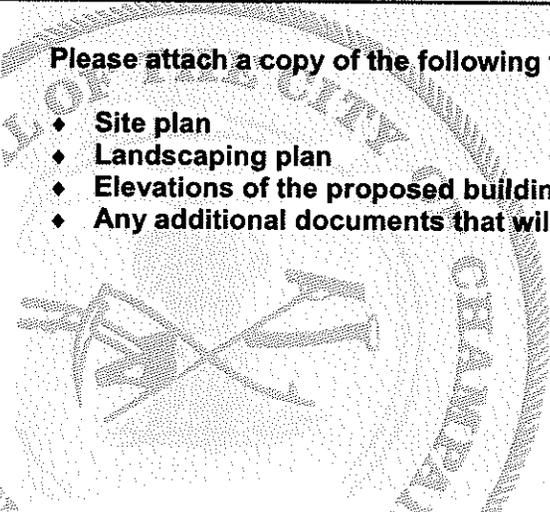
Building Heights: 80 feet

\*Includes 131 public parking spaces

Development Schedule: Commence construction spring 2014 with completion summer 2015.

Please attach a copy of the following to the to this application:

- ◆ Site plan
- ◆ Landscaping plan
- ◆ Elevations of the proposed buildings
- ◆ Any additional documents that will aid in the review of the proposed project



**The proposed project must meet the following criteria for approval as described in Section 37-306 if the Champaign Zoning Ordinance:**

- (1) The proposal advances the purpose statements of Article VI of the Zoning Ordinance.

See attached

- (2) The proposal meets the minimum development requirements of Article VI of the Zoning Ordinance.

See attached

- (3) The proposal adequately and appropriately incorporates the design guidelines in Table VI-D that are appropriate to the type of development being proposed.

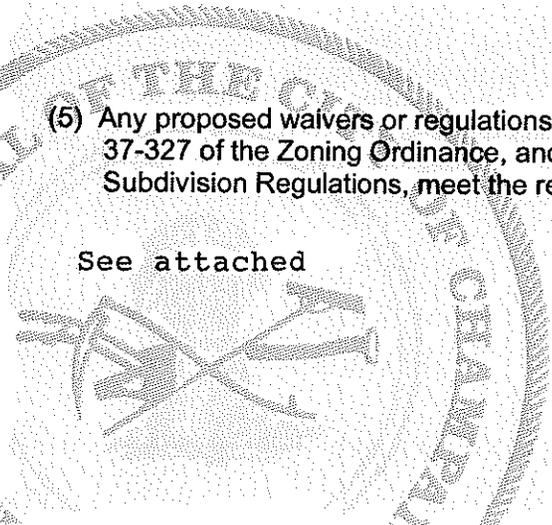
See attached

- (4) The proposed zoning is consistent with the Comprehensive Plan and is in the best interest of the public.

See attached

- (5) Any proposed waivers or regulations or requirements of this Chapter meet the requirements of Section 37-327 of the Zoning Ordinance, and any proposed waivers of Chapter 31, the Champaign Subdivision Regulations, meet the requirements of Section 37-328 of the Zoning Ordinance.

See attached



List any waivers of required items that are part of the proposal and justification waivers (attaché separate sheet if necessary).

Zoning Ordinance Waivers (see Section 37-327 of the Zoning Ordinance):

See attached

Subdivision Regulation Waivers (see Section 31-106):

See attached

Are there any elements of the Planned Development which are based on any flexible requirements that are permitted in this article? Please provide details on a separate sheet if necessary.

N/A

Does the applicant intend to sell or lease all or a portion of the Planned Development after it is approved?

YES

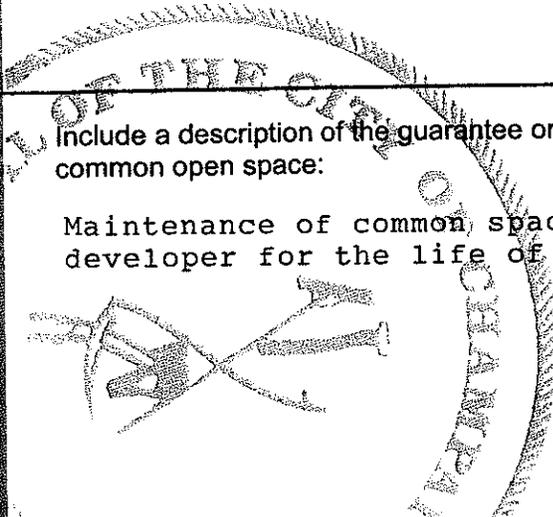
NO

If yes, describe the conditions of sale and maintenance of such properties and include a general description of any deed restriction, covenants, and similar agreements:

Applicant will lease apartments and retail space.

Include a description of the guarantee or covenants of ownership to be used for the maintenance of any common open space:

Maintenance of common space will remain the responsibility of the developer for the life of the project.



Review Fees:

Preliminary Application (Per Acre):	\$15.00
Public Hearing:	\$30.00
Public Notice:	Direct Cost

Signature(s) of the petitioner:

*J. Bell*

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Date:

*12/13/13*

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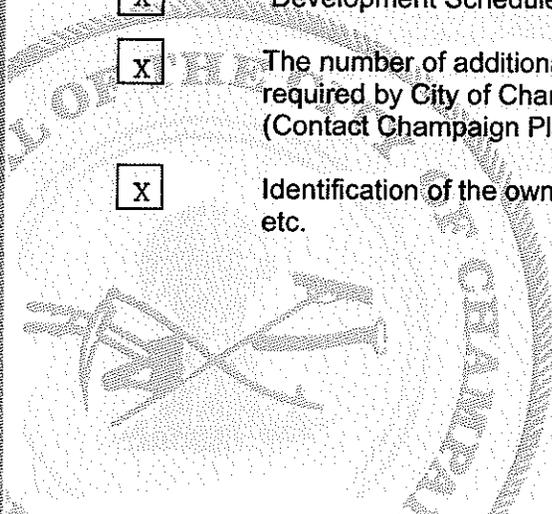
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The following items must be attached to the completed application before it will be considered for review.

- Proof of ownership (attach copy of title, deed, etc.) by development agreement
- Proof of Authorization (if the owner(s) of record are different than the applicant(s)).
- One copy of all supporting materials.
- Name of owners, including Statement of Beneficial Interest. This information must be listed on the application, but is not required on the face of the plat.
- Review fees as required in **City of Champaign Municipal Code Section 26-16**. Check made payable to the City of Champaign.
- Waiver Request Letter shall include all new waiver requests and justification for the waiver item(s) in accordance with these regulations and standards.
- Development Schedule, if requested.
- The number of additional copies and the sheet size for preliminary submittals as required by **City of Champaign Municipal Code Manual of Practice Section 2.03** (Contact Champaign Planning Department for further information).
- Identification of the owner's representative—development manager, engineer, surveyor, etc.



**Owner Authorization Form**

If the applicant is not the owner of the property, please have the owner(s) or owner(s)' agent sign the appropriate section and have the signature(s) notarized to authorize said applicant to process the application.

INDIVIDUAL

**Section 1. Owner(s) is/are Individual(s)**

The undersigned hereby state(s) that she/he/they is/are the owner(s) of the property that is the subject of the forgoing application for a Preliminary Planned Development, that she/he/they has/have read said application, and that she/he/they hereby authorize(s) \_\_\_\_\_ to act as her/his/their agent with regards to the processing of said application for the purposes set forth herein.

First Owner's Name (printed or typed): \_\_\_\_\_

First Owner's Signature: \_\_\_\_\_

Second Owner's Name (printed or typed): \_\_\_\_\_

Second Owner's Signature: \_\_\_\_\_

PARTNERSHIP

**Section 2. Owner is a ~~Partnership~~ LLC**

The undersigned hereby states that the partnership of Campus Center, LLC is the owner of the property that is the subject of the forgoing application for a Preliminary Planned Development, that he/she is one of the partners of said partnership, that he/she has read said application, and that he/she hereby authorizes John Scott Kunkel to act as the partnership's agent in processing said application for the purposes set for the herein.

~~Managers~~  
~~Partners~~ Name (printed or typed): Christopher M. Hartman

~~Managers~~  
~~Partners~~ Signature: [Signature]

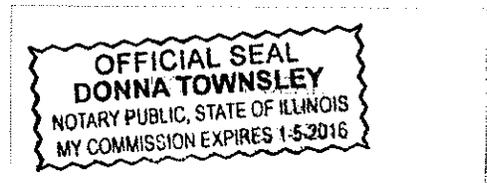
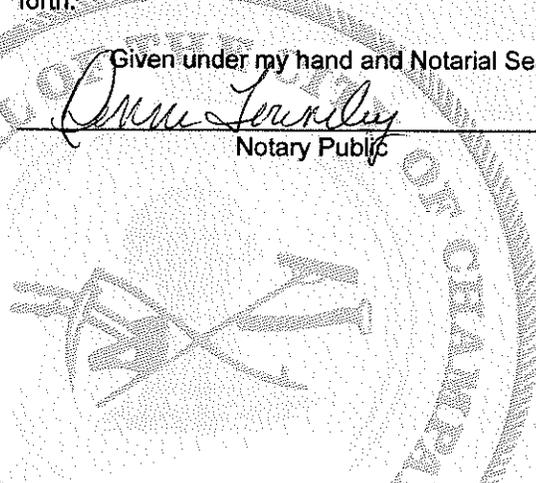
NOTARY

STATE OF Illinois )  
COUNTY OF Vermilion ) ss.

I, the undersigned, a Notary Public, in and for said County, in the State aforesaid, DO HEREBY CERTIFY THAT Christopher M. Hartman is/are personally known to me, that said person(s) appeared before me this day in person and severally acknowledged that he/she/they signed and delivered the forgoing Owner Authorization Form as his/her/their free and voluntary act for the uses and purposes therein set forth.

Given under my hand and Notarial Seal this 13 day of December 2013.

[Signature]  
Notary Public





# PRELIMINARY PLANNED DEVELOPMENT APPLICATION SUPPLEMENTAL INFORMATION

**CAMPUS CENTER**  
**524 E. GREEN STREET & 601 S. SIXTH STREET.**

Application Page 2:

## *Legal Description*

Lots 1, 2, 3, 11, and the east 38 feet of Lot 10, all in Block 8 of the Subdivision of Blocks 5, 7, 8, and 9 of J.S. Wright's Addition to the City of Champaign, Champaign County, Illinois.

Application Page 6:

## *Approval Criteria*

- (1) *The proposal advances the purpose statements of Article VI of the Zoning Ordinance.*
- (a) *Allow flexibility that is not available through standards and restrictions contained elsewhere in this chapter.*

While the proposed development largely adheres to the prescriptive requirements of the Zoning Ordinance, some flexibility is required to provide the desired use mix on a uniquely configured site within the context of the guidelines and restrictions contained in the Request from Proposals.

- (b) *Encourage infill development and promote the redevelopment potential of under-utilized central city sites.*

The proposed development is clearly an infill style development and creates a more appropriate use mix and development density on a central Campustown site.

- (c) *Promote more efficient use of land.*

The density of the proposed development provides a more efficient use of land and infrastructure resources by locating residents and customers proximate to their daytime destinations. The inclusion of a structured parking component provides a more efficient land use for parking needs for the project and surrounding area.

- (d) *Incorporate site features such as topography, views, vegetation, water features, and other factors into the design so they become assets to the development.*

The urban context of the site naturally minimizes the availability of the noted site features. The primary site feature is its central Campustown location affording opportunities for more efficient interplay between living, education, and work activities.

*(e) Provide additional amenities that would not otherwise be required under conventional zoning.*

The primary amenity provided by the development is the provision of a new and unique use to Campustown with the inclusion of a hotel in the project. Also significant is the substantial public parking component with at least 131 spaces available for public use. Another amenity is the extension of the Boneyard Creek Linear Park to Sixth Street.

*(f) Promote building styles and architectural styles that complement one another.*

The building and architectural style are consistent with the current fabric of recent development in the Campustown area. The scale of architectural elements and anticipated materials are consistent with this part of the community. The massing strategies maintain a pedestrian scale at street level while still accommodating the desired infill density for the site.

*(g) Allow a mix of uses that are designed to negate potential conflicts that normally occur between incompatible land uses.*

The proposed uses are compatible with each other and with the surrounding area. The inclusion of a hotel reduces conflict by locating visitors proximate to their final destinations, minimizing the need for extra vehicular traffic in and out of the Campustown area. The retail component of the project enhances selections for consumers and further reduces the need to travel away from their home neighborhoods in search of services and amenities. The residential component provides opportunities for people affiliated with the University to live proximate to their classes or workplace while also being close to dining and retail options with limited need for private transportation. Structured parking eliminates the visual impact of surface parking and provides more efficient access and more flexible parking options for customers accessing Campustown.

*(h) Promote the most efficient arrangement of circulation systems, land use, and buildings.*

The site layout is configured to optimize vehicular ingress and egress for the parking deck and maintain direct pedestrian access to the retail uses fronting on Green Street and Sixth Street. Ground level lobby footprints are minimized for the hotel and residential uses, while still providing convenient access to building occupants. The formal extension of the Boneyard Creek Linear Park provides a clear path for pedestrians to complete their transit to the core of Campustown from areas west of the development. The orientation of the hotel lobby also serves to enhance the vitality of the Linear Park and avoid the appearance of the “rear” of a building.

*(i) Promote environmentally sensitive developments.*

The project is programmed to pursue LEED Silver classification from the U.S. Green Building Council. This classification is indicative of a high level of environmental sensitivity in the overall project.

*(j) Allow development, under a specifically approved design concept and site plan, which otherwise may not be permitted by the Zoning Ordinance*

In order to provide the desired uses and density while adhering to the supplemental requirements of the Request for Proposals other stated City preferences, some relief from the Zoning Ordinance and the Campustown Commercial Overlay District guidelines is required. These issues are identified in the Zoning Ordinance Waivers section below.

(2) *The proposal meets the minimum development requirements of Article VI of the Zoning Ordinance.*

Where applicable, the proposal meets the proscribed minimum development requirements except as specifically noted in the Zoning Ordinance Waivers section.

*(a) Yard. Yards along the periphery of a Planned Development shall be provided as required by the regulations of the district in which said development is located.*

Designated yards are provided with the specific exception of areas noted in the Zoning Ordinance Waivers section.

*(b) Parking. The number of parking spaces required for commercial and industrial uses in a Planned Development shall be calculated in accordance with the provisions for collective parking in Article VII of this chapter.*

Parking capacity is in excess of the Zoning Ordinance requirement when analyzed from a market demand viewpoint.

*(c) Signs. Signs for businesses in a Planned Development with underlying residential zoning shall comply with the sign regulations for the CO District. All other signs and sign structures must comply with the regulations for the district in which such Planned Development is located.*

All signage will comply with the CB District requirements with the exception of hotel signage allowance noted in the Zoning Ordinance Waivers section.

*(d) Street construction. Street construction, regardless of ownership, shall be made in conformance with the Subdivision Regulations. Minimum pavement widths for private drives servicing more than one structure when authorized, shall be thirteen (13) feet for the first lane of traffic in each direction and eleven (11) feet for each additional lane. Street construction plans and details shall be submitted to the City Engineer for review and approval prior to final plan approval.*

No new street construction is anticipated as part of this project.

*(e) Multiple structures. There may be more than one main building on a lot in a Planned Development. Buildings to be used for multiple dwellings, institutional, or other purposes and shall comply with the spacing requirements of the chapter.*

The mix of project uses is configured in a manner to be segregate building massing by use type, reducing the potential for conflict resulting from differing use groups and usage patterns.

*(f) Public improvements. All public improvements shall meet the requirements of the Subdivision Regulations. A waiver of the requirements for public improvements may be granted as part of the Planned Development approval when criteria in the Subdivision Regulations for granting a waiver have been met.*

No subdivision related work is envisioned as part of this project.

*(3) The proposal adequately and appropriately incorporates the design guidelines in Table VI-D that are appropriate to the type of development being proposed.*

Where applicable, the proposal adequately and appropriately incorporates the Table Vi-D design guidelines.

### *1.0 SITE PLAN GUIDELINES*

*Purpose: Well designed site plans create a sense of internal cohesiveness and compatibility with the surrounding property. The site plan shall be designed to take into account all internal and external physical features to create an arrangement of and relationship between all structures on the site, which minimizes impacts from or on surrounding land uses and which promotes a sense of cohesiveness for the development.*

*1.1 Dumpsters, central air units, utility boxes, and other similar features should be located in inconspicuous locations and screening should be provided so that they are not visible from public right-of-way or adjacent property.*

The indicated items are located in core areas of the site with appropriate screening, limiting visibility from public right-of-ways and adjacent property. Residential garbage is collected and stored in an enclosed room, further reducing its impact.

*1.2 Individual buildings should be integrated into the overall site plan through joint parking, pedestrian connections, plantings, shared access, and location of freestanding buildings so as not to block visibility of the larger commercial center.*

The overall project shares as an internal circulation pattern to provide convenient access to all building areas. Access between buildings is accomplished by leveraging the extended linear parkway.

*1.3 Adverse off-site impacts should be minimized through building orientation and provisions for appropriate transition areas such as a landscaped buffer, park, berm, or combination of treatments.*

Generally not applicable given urban environment.

*1.4 Buildings should be sited and grouped in a coherent layout based on a site analysis that considers topography, views, privacy, drainage, function of buildings, and other design guidelines contained herein.*

The mix of project uses is configured in a manner to organize massing in response to usage. The arrangement of uses is intended to optimize views, privacy, and overall building function. Drainage will be improved through directed collection and routing to existing storm infrastructure. No new storm drainage load will be created by the development.

## 2.0 TRAFFIC AND CIRCULATION

*Purpose: The layout of the circulation system is basic to the design of a Planned Development. The traffic and circulation system shall provide for efficient, safe, aesthetic movement of pedestrians and vehicles, through and adjacent to the development.*

*2.1 There shall be adequate access for emergency vehicles, garbage collection, and delivery vehicles.*

The east-west access alley and west bypass lane provide adequate access for the indicated use.

*2.2 The development shall be designed with the minimum number of access points to public streets necessary to serve the traffic generated by the proposed use.*

Access points were proscribed by and are consistent with the Request for Proposals.

*2.3 Safe and convenient multi-use sidewalks or paths should be provided from residences to recreation areas, bus stops, parking areas, and community buildings within the Planned Development.*

The urban nature of the development directs these uses to the public sidewalks bordering the site. The project does include extension of the Boneyard Creek Linear Park to Sixth Street.

*2.4 Design of the internal street system should be coordinated with the existing and future street system and conform to the Transportation Element of the Comprehensive Plan. Continuation of collector streets shall be provided for through the site in accordance with the Comprehensive Plan.*

No internal streets are included in the project scope.

*2.5 Parking facilities for bicycles should be provided at convenient locations.*

Bicycle parking facilities are provided within the grade level parking area in quantities consistent with Zoning Ordinance requirements. These areas are readily accessible to building users and the public and offer the advantage of covered bike storage.

## 3.0 ARCHITECTURAL QUALITY

*Purpose: Planned Developments shall include architectural design which results in a cohesive design compatible with existing buildings on or around the site.*

*3.1 Multiple buildings in a Planned Development should relate to one another through common architectural styles, scale, building materials, and signage.*

The mix of project uses is configured in a manner to be perceived as a single complex through complementary materials, massing, and design elements.

*3.2 Architectural design should create visual interest through textures, complementary colors, and attractive facades.*

The architectural design is intended to create a pedestrian scale development while still accommodating the needs of a large scale development. Materials and colors are utilized to support this effort.

*3.3 Buildings and signage should be sensitive to the style, period, and scale of neighboring properties.*

The building style and associated retail signage are consistent with the existing character of the Campustown district.

*3.4 Street furniture such as light posts, trash receptacles, and benches shall be functional and compatible with the architectural design.*

Street furnishings are already provided through the adjacent existing streetscape development.

*3.5 Where large structures are required, mass should be broken up through setbacks, building offsets, varied story heights, and other design techniques.*

Building mass is mitigated through varied setbacks at the second and third levels and a modulation of building facades to create offsets. A vertical hierarchy of architectural elements also contributes to the perceived reduction in building mass.

*3.6 Commercial centers should reflect the human scale at street level through use of entryways, windows, columns, awnings, and other techniques.*

Expansive glazing, pedestrian scaled elements, sidewalk oriented entries, and awnings all assist in maintaining a human scale at street level.

*3.7 Use of art features such as fountains, special landscaping, sculpture, and murals that lend identity to a development should be utilized.*

The urban infill nature of the site development leaves little site area for the indicated site features.

*3.8 Walls and fences should be compatible with site architecture and should be unobtrusive to neighbors.*

No significant wall or fence construction is anticipated as part of the project.

3.9 All buildings shall utilize finished building materials on all sides.

Finished building materials are provided on all sides.

#### 4.0 ENVIRONMENTAL GUIDELINES

*Purpose: Planned Developments shall be constructed with regard for the environment to avoid adverse impacts on existing natural features, drainage, erosion, water quality, and energy conservation.*

*4.1 Drainage features should be integrated into the development and treated as an asset by providing aesthetically pleasant open space and multi-use facilities.*

The urban infill nature of the development necessitates direct collection of storm water.

*4.2 Floodplains, drainage features, and natural areas should be preserved and enhanced as natural environments.*

Not applicable given prior municipal infrastructure work rerouting natural drainage below grade.

*4.3 Adequate setbacks and access to stormwater facilities and drainage ways should be provided in order to encourage dual use as recreation, habitat, and storage area for excessive rainfalls and to provide for future maintenance.*

Not applicable given prior municipal infrastructure work rerouting natural drainage below grade.

*4.4 Preservation and addition of vegetation should be provided along stream and drainage ways to prevent erosion, provide natural habitats, and improve water quality.*

Not applicable given prior municipal infrastructure work rerouting natural drainage below grade.

*4.5 Significant trees of good health should be protected from damage during construction and from future land uses in the development whenever possible.*

There are few existing mature trees within site boundary and they are positioned in places that are inconsistent with the infill development footprint.

*4.6 Building layout should consider sun angles, prevailing winds, shade, and other factors to save energy costs.*

These issues are addressed in a holistic manner as part of the LEED certification process.

#### 5.0 OPEN SPACE GUIDELINES

*Purpose: Planned Developments shall provide for both the preservation of natural areas and the creation of areas for active or passive recreation.*

*5.1 Where space permits, a development should incorporate a variety of open space including areas for active recreation, passive recreation, and undeveloped natural areas.*

The urban infill nature of the development relies upon existing recreation options within the district.

*5.2 Undeveloped open space should preserve healthy vegetation and remove dead or diseased trees.*

Not applicable. No pre-existing undeveloped open space exists within the site boundary.

*5.3 Play areas for children should be close to building entrances and should be located so children do not have to cross high traffic areas.*

Not applicable. The residential market for this area generally precludes the presence of children.

*5.4 Active recreation facilities provided on site should reflect the age of the residents, consider existing nearby public facilities, and provide adequate area and dimensions for the use consistent with national standards.*

The urban infill nature of the development relies upon existing recreation options within the district. These amenities are in abundance and fully developed within the context of the University of Illinois.

*5.5 Passive recreation areas should be graded and seeded or have other ground cover. Such areas should include trails, picnic areas, and take advantage of vegetation, shade and views, and be removed from nuisance areas.*

Not applicable. The urban infill nature of the development limits the provision of passive recreation areas.

*5.6 Open space areas should be spread out to serve the entire development and should be connected through trails or common yards.*

Not applicable. The urban infill nature of the development limits the provision of open space.

*5.7 Water detention areas may comprise a portion of the open area, but should not represent the majority of open space and should be usable by the residents.*

Not applicable given prior municipal infrastructure work rerouting natural drainage below grade.

## **6.0 PRIVACY/SECURITY GUIDELINES**

*Purpose: As an incentive, Planned Developments may be built at a higher density than conventional developments. Allowing higher density while maintaining privacy for residents is a goal. Planned Developments shall consider security as a design feature.*

*6.1 Residential units should be oriented and spaced to promote privacy.*

Residential units are oriented with views toward exterior spaces and generally include views off-site. Construction techniques and unit arrangement within the building help provide privacy for residents.

*6.2 Entrances to buildings and parking areas shall be well lighted.*

All entrances and parking areas will be well lit with photocell controlled fixtures to provide dusk to dawn illumination.

*6.3 Public spaces should be designed to be visible from dwelling units and promote a sense of ownership for surveillance and security purposes.*

All public spaces are visible from dwelling units and most spaces are also visible from retail areas.

*6.4 Remote entrances to public spaces not directly visible from neighboring units and the establishment of secluded areas should be avoided.*

All primary entrances to public spaces are from public sidewalks with direct visibility from retail and residential uses.

*6.5 Dwelling units should be oriented or screened to avoid glare from headlights.*

All dwelling units are on the second floor or above, eliminating the possibility of glare from vehicular headlights.

## **7.0 LANDSCAPING AND SCREENING GUIDELINES**

*Purpose: Landscaping of the Planned Developments shall be designed to provide for adequate landscaping and effective screening for off-street parking areas and for areas where nonresidential use or high density residential use may be detrimental to lower density areas. Landscape materials shall be integrated into the overall scheme of the site and should compliment the other physical features of the area.*

*7.1 Landscaped buffers required along the perimeters between incompatible uses shall be integrated into the overall landscaping for the development.*

Not applicable. The urban infill nature of the development limits the provision of landscape buffers.

*7.2 Selection of plant materials shall be in conformance with the Approved Plant List which is available for inspection in the office of the city clerk.*

The urban infill nature of the development limits the provision of landscape materials. The limited planting areas will conform to the Approved Plant List.

*7.3 The selection and location of landscape materials in utility corridors shall be such that at their mature height, trees shall not interfere with overhead utilities and streetlights.*

The urban infill nature of the development limits the installation of trees within the site boundary. The minimal landscaping anticipated will not conflict with utility corridors.

*7.4 Clusters of trees and bushes or flowerbeds should also be used throughout the site to provide visual relief.*

The urban infill nature of the development limits the installation of trees, bushes, and/or flowerbeds within the site boundary. The landscaping provided is clustered.

*7.5 Street trees along public streets and private access drives should be provided at a ratio of at least one tree per sixty (60) feet of frontage.*

Street trees are already provided through the adjacent existing streetscape development.

*7.6 In addition to required landscaped islands in parking lots, other treatments such as planters, flower beds, and decorative rock or stone should be considered to provide relief in larger impervious areas.*

Not applicable. All project parking is within a covered, multi-level parking deck.

*7.7 Landscaping should be provided at site entrances, public areas, and adjacent to buildings to provide visual interests and screen mechanical equipment.*

Not applicable. The urban infill nature of the development limits the provision of landscape materials. All mechanical equipment is located in interior site locations, limiting the need for screening from public right-of ways.

*7.8 Plant material shall be maintained in such a manner as to preserve their functional aesthetic integrity.*

Not applicable. The urban infill nature of the development limits the provision of landscape materials.

*7.9 Whenever possible, existing mature trees on the site should be incorporated into the overall landscape design and preserved as part of the development.*

There are few existing mature trees within site boundary and they are positioned in places that are inconsistent with the infill development footprint.

*(4) The proposed zoning is consistent with the Comprehensive Plan and is in the best interest of the public.*

The proposed project is consistent with the Comprehensive Plan with its focus on infill development in the existing service area as well as the specific uses envisioned.

*(5) Any proposed waivers or reductions in requirements of this Chapter meet the requirements of Section 37-327 of the Zoning Ordinance, and any proposed waivers of Chapter 31, the Champaign Subdivision Regulations, meet the requirement of Section 37-328 of the Zoning Ordinance.*

Where applicable, the proposal meets the requirements of Section 37-327 the Zoning Ordinance. No waivers of Chapter 31 are requested.

*(1) The proposed waiver is in harmony with the general purpose and intent of this Chapter;*

Consistent with the general purpose of the Planned Development Chapter, the proposed waivers are associated with the redevelopment of an urban infill site with challenging site constraints and additional requirements associated with municipal goals for the development. The development addresses a unique market niche with the addition of the hotel use and an emphasis on smaller bed count residential units. It provides an efficient land use of an otherwise underutilized site.

*(2) The proposed waiver will not alter the essential character of the locality;*

As the Campustown District is already characterized by a dense level of infill development with consistent and complementary uses, the proposed waivers will not alter the essential character of the locality. In many ways, the proposed development will amplify and improve the essential character of the locality.

*(3) The proposed waiver will not impair an adequate supply of light and air to adjacent property, substantially increase the congestion in the streets, increase the danger of fire or crime, diminish the value of nearby property, nor impair the public health, safety, comfort, convenience, or general welfare.*

The proposed waivers will not impair an adequate supply of light and air to adjacent property as the waivers are generally unrelated to areas of the development facing adjacent property. Setbacks of upper floors from adjacent property lines are often well in excess of the proscribed minimum setbacks. Street congestion will not increase with the deliberate routing of traffic away from the overused Green Street corridor. Parking ingress and egress will not compete with already heavily used streets. The development will include all code required fire prevention and life safety features to maintain the greatest possible protection against the danger of fire. The increased presence of people and activity throughout all times of the day will reduce the attractiveness of crime and increase the sense of security for the public. The proposed development will enhance nearby property values with the added customer base and the improved options for public parking. Public health, safety, comfort, convenience, and general welfare will all benefit from the additional diversity of services and residential uses as well as from the indirect benefit of expanded property tax, hotel/motel tax, food and beverage tax, and sales tax revenue.

Application Page 7:

*Zoning Ordinance Waivers:*

- A. The RFP process suggested a focus on increasing project density with the requisite diminishment of reliance on open space. In addition, a significant portion of the residential area is associated with a hotel use for which open space is not typically provided in an urban hotel configuration. The project is not designed to meet a specific open space requirement. Total available grade level open space is approximately 2,900 square feet. The project also anticipates approximately 2,090 square feet of interior recreation space assigned to a pool and fitness area to be shared by the hotel and residential uses and approximately 1,740 square feet of courtyards spread across four distinct spaces on the second and fifth levels.
  
- B. 71 parking spaces are required by the Zoning Ordinance for the residential use (81 spaces less the 10 space reduction per 37-358(b)(2)) and an additional 131 public parking spaces were requested by the City of Champaign to provide public parking, for total of 202 spaces. Approximately 176 spaces are provided in the proposed parking deck. Our experience with the market indicates actual residential demand is approximately 40% of the zoning required parking for the type of units provided. In that scenario, market required parking for the residential component of the project would reduce to 29 spaces. Similarly, the hotel management consultant has indicated the urban setting will significantly limit demand for hotel parking.

The developer controls over 400 parking spaces within a one block radius. With currently over 120 vacant spaces among these parking areas, any excess demand from the new project will be easily absorbed with existing capacity, easily allowing the project's parking facility to provide a minimum of 131 spaces for public use. It is also important to note the 131 public parking spaces reflect an increase of 46 spaces from the actual number of hourly parking spaces currently provided by Lot J. The balance of the current Lot J spaces is assigned/leased parking spaces which can be readily absorbed into excess market capacity.

*Campus Commercial Overlay District Performance Standard Waivers:*

Section designations refer to the applicable performance standard. Project complies with all standard not noted.

- c(4) Project complies with the assumption this requirement is not applicable to the portion of the parking garage facing Healey Street and the opening to the drive and Boneyard Parkway facing Sixth Street.
- c(5) Given the height of the first floor (20+ feet), building setbacks generally commence at the second floor in lieu of the third floor.

The Green Street façade sets back the requisite 5 feet at the third floor with the exception of two modules, each approximately 18' wide, that help define the linear character of the façade. These two modules maintain the grade level façade plane over the full height of the building. The grade level façade plane varies between 3 feet and 4 feet from the property line. One other exception to the set back is the main stair at the east end of the Green Street façade. Given the need to maintain a continuous stair enclosure over the full height of the building, no upper level setback is provided.

Due to the minimum site area necessary to accommodate a parking garage, the portion of the building on the northeast quadrant of the site has an atypically narrow footprint, limiting the ability to achieve a full 5 foot setback of upper levels from the lower building façade. Thus, the Sixth Street façade sets back 3'-4" beginning at the second floor with the exception of one module approximately 20' wide that help define the linear character of the façade and serves as a massing cue for the building entry location. This module maintains the grade level façade plane over the full height of the building.

Beginning at the second floor line, the upper floors of the Healey Street façade set back approximately 3 feet from the lower building façade, but maintain a 6 foot setback from the property line. One other exception to the set back is the main stair at the west end of the Healey Street façade. Given the need to maintain a continuous stair enclosure over the full height of the building, no upper level setback is provided.

Given the structural and functional restrictions of the parking deck, no additional setback is anticipated for upper levels of the parking deck's Healey Street façade.

- c(6) Project provides approximately 74% glazed façade at grade level along Green Street, approximately 77% glazed façade at grade level along Sixth Street, and approximately 75% glazed façade at grade level for the retail portion along Healey Street.

A specific exception to this requirement is the parking deck for which this requirement would not be reasonably applicable and the building service space portion of the narrow Healey Street frontage for which windows are not practical. Stair towers are excluded from the frontage calculation.

- c(7) Project provides approximately 36% glazed façade for upper levels along Green Street, approximately 32% glazed façade for upper levels along Sixth Street, and approximately 24% glazed façade for upper levels for the residential portion along Healey Street. The Green Street façade upper level glazing maximizes the glazing allowed within franchise design requirements for the Marriott hotel product. The Sixth Street and Healey Street upper level façade glazing provides the maximum reasonable amount of glazing to remain consistent with the punched window motif of the architectural style.

A specific exception for this requirement is the Healey Street façade for which this requirement would not be reasonably applicable given the inclusion of the parking deck. Also excluded are portions of the building facing a street but not actually fronting on the street. Excluding the elevations not fronting on a street is reasonable for this project given (i) there are existing buildings between the referenced elevations and the street, largely screening the elevations from view; and (ii) the referenced elevations set back from the property line between 70 feet to 100 feet, mitigating their perceptual impact on the overall façade. Stair towers are excluded from the area calculation.

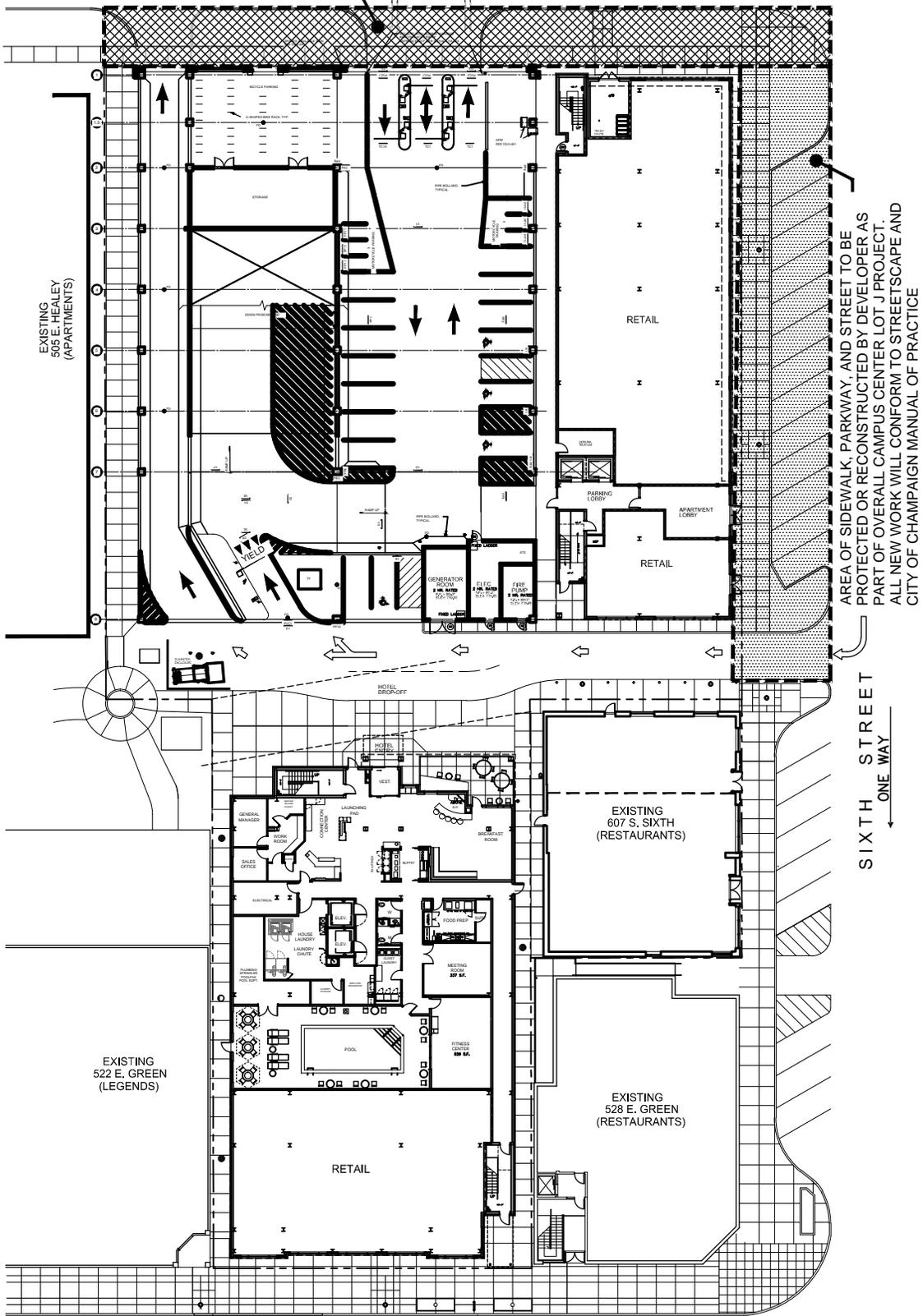
- c(11) The parking deck maintains a minimum 10 foot setback from adjacent property lines with the exception of property lines shared with a public right-of-way.
- c(12) The Healey Street facing façade of the parking deck will provide partial screening at guardrail height. This height is tall enough to screen vehicle headlights from projecting out of the deck. The balance of the wall will be open to provide the building code required ventilation necessary for an open parking structure.

*Subdivision Regulation Waivers:*

No subdivision regulation waivers are required.

AREA OF SIDEWALK, PARKWAY, AND STREET TO BE COORDINATED BY FOTH AS PART OF OVERALL HEALEY STREET RECONSTRUCTION PROJECT.

HEALEY STREET  
ONE WAY

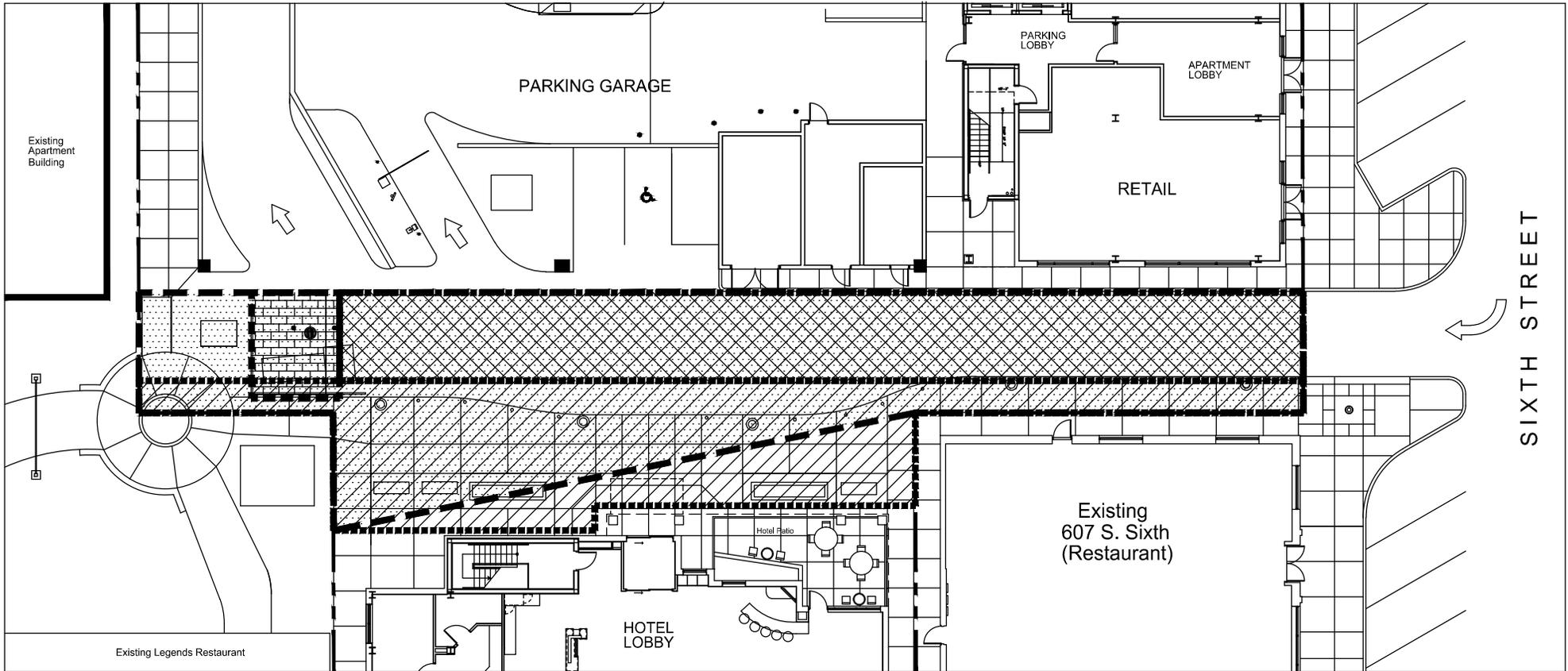


GREEN STREET



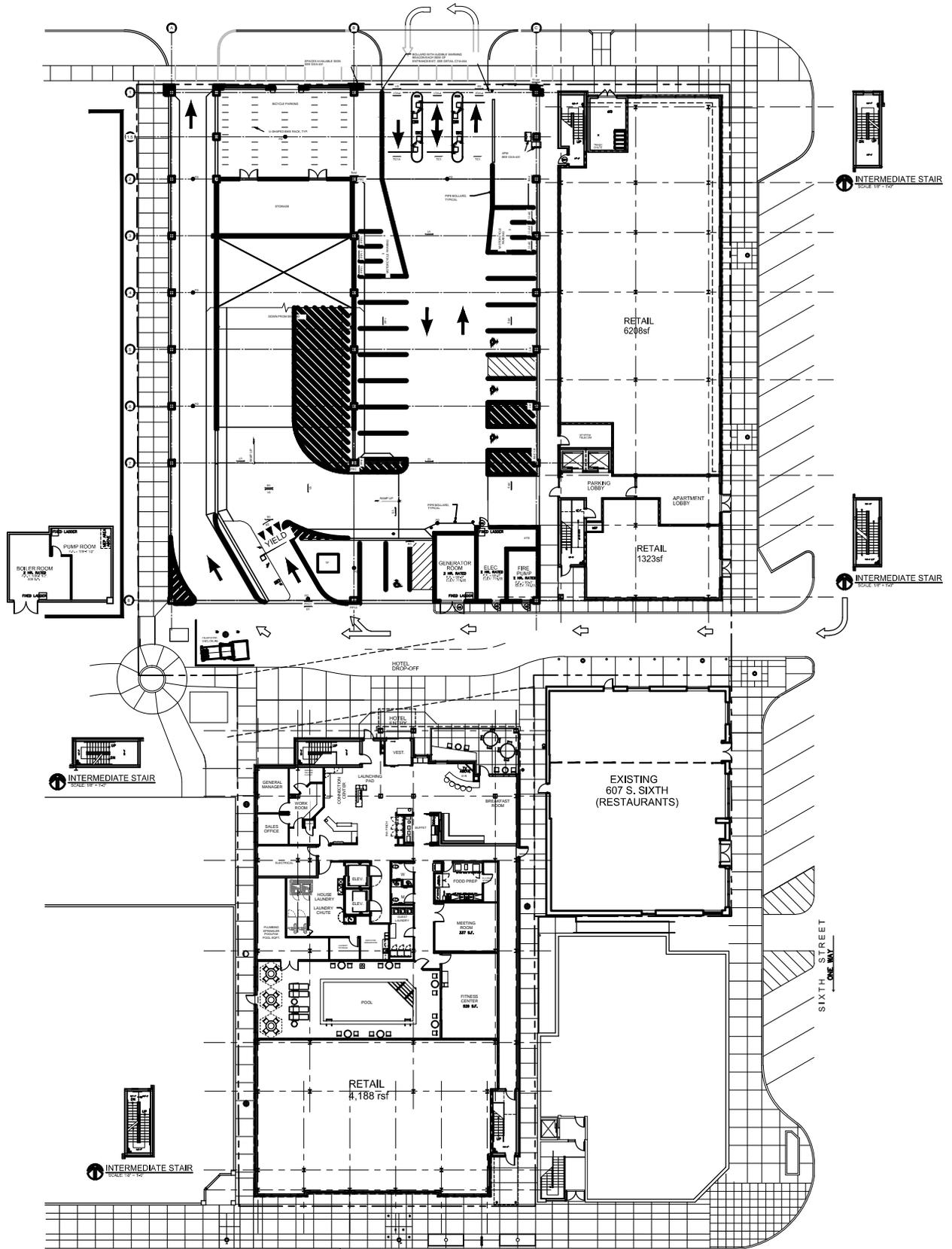
CAMPUS CENTER  
SITE PLAN  
NOT TO SCALE

SIXTH STREET  
ONE WAY



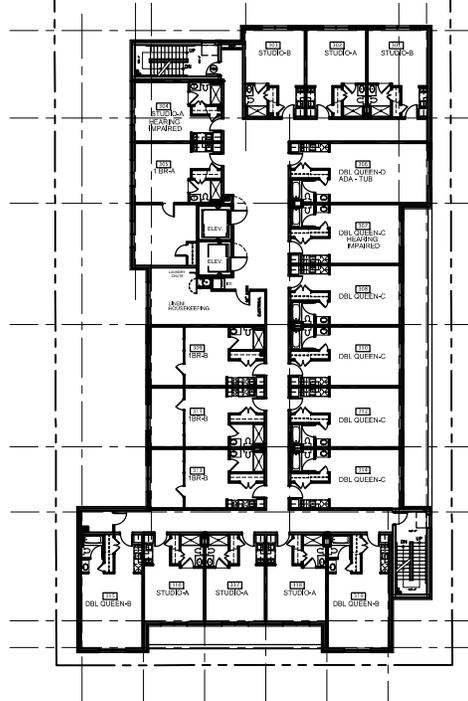
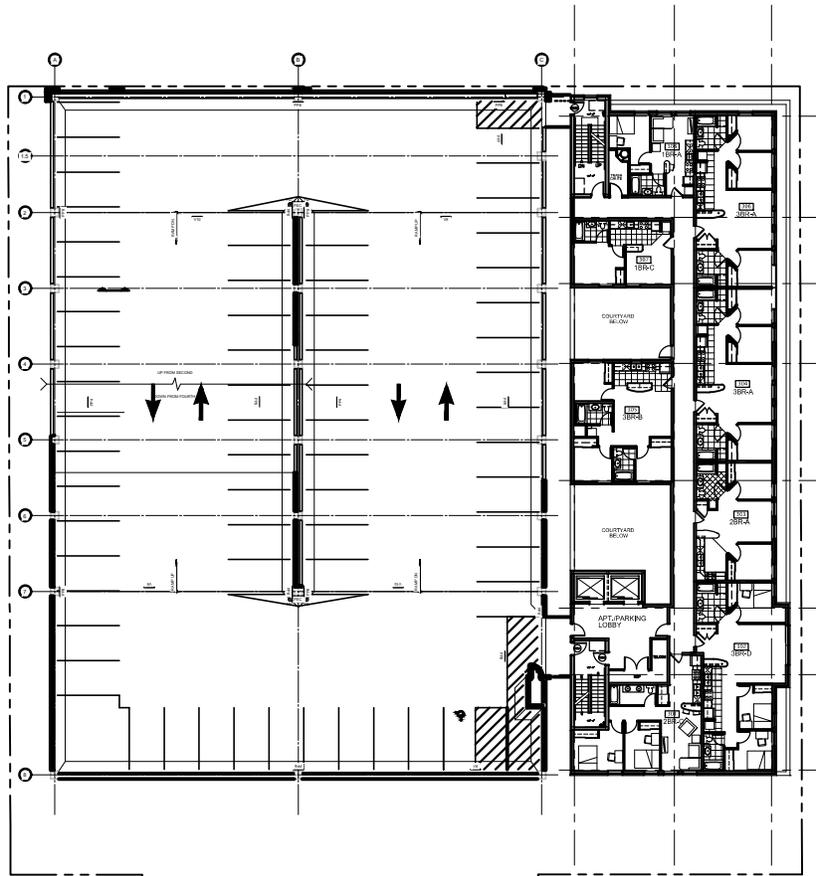
**Boneyard Greenway Extension - Easement Summary**

-  Vehicle Access Easement
-  Boneyard Stormwater & Utility Easement
-  Multi-Use Trail Easement
-  Dumpster Easement



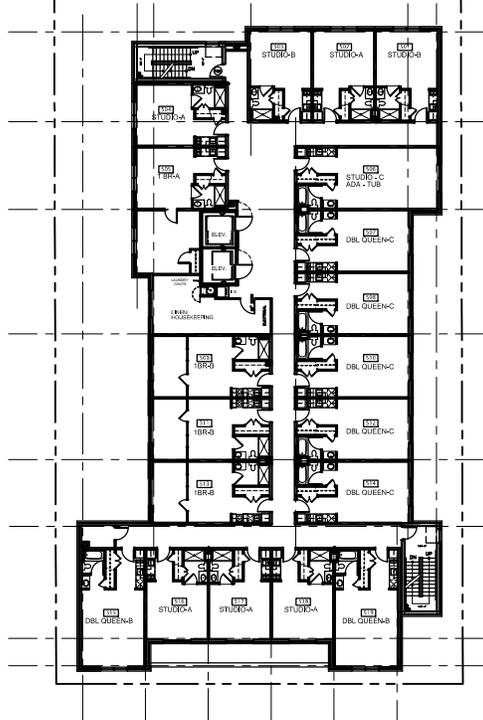
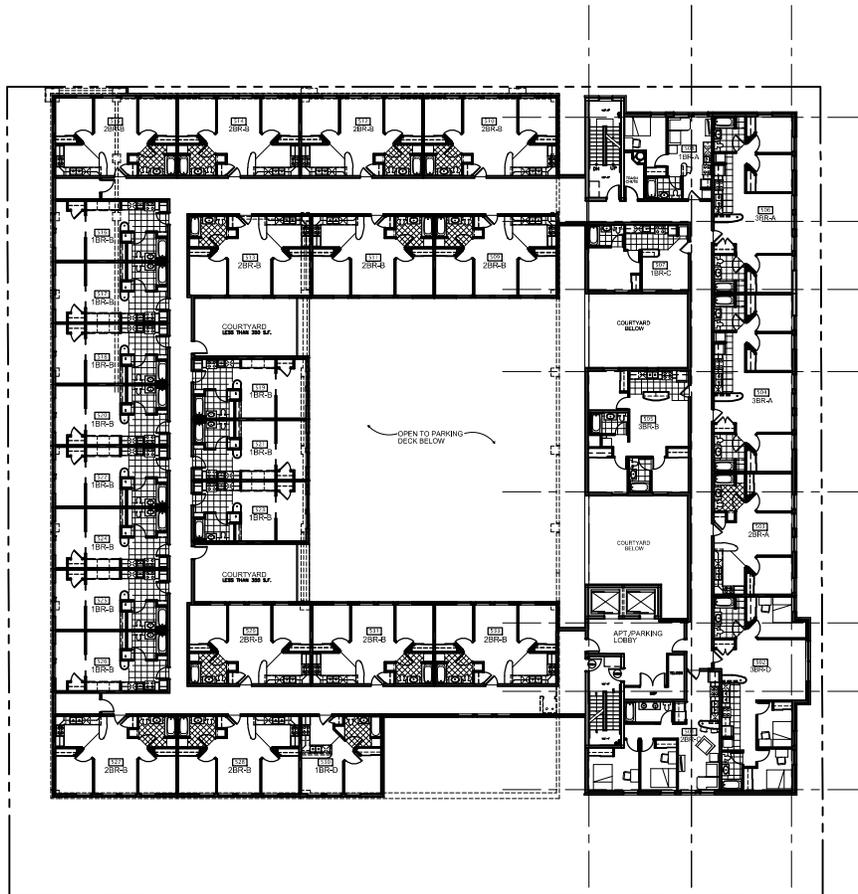

**CAMPUS CENTER**  
**1ST FLOOR PLAN**  
 SCALE: 1/16" = 1'-0"



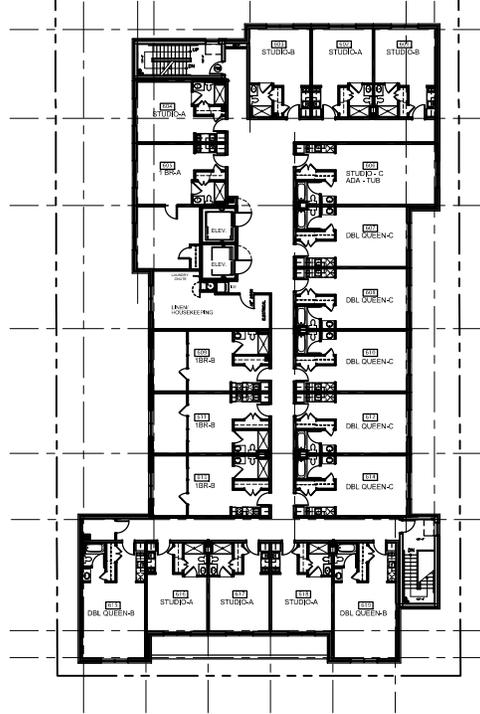
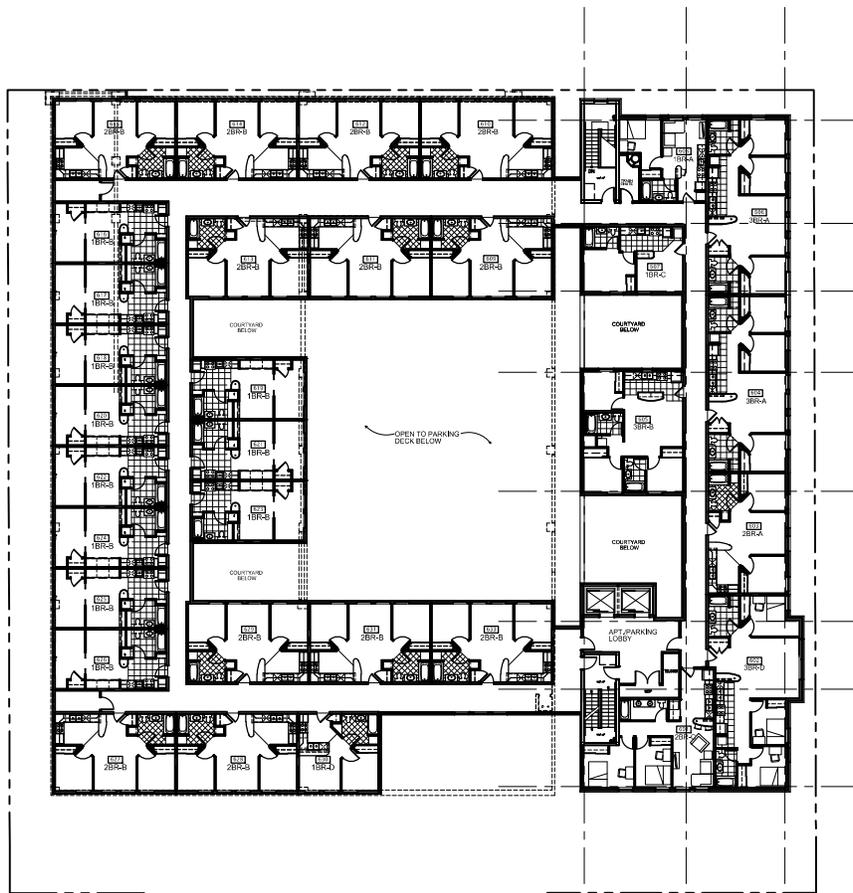



**CAMPUS CENTER**  
**3RD FLOOR PLAN**  
 SCALE: 1/16" = 1'-0"






**CAMPUS CENTER**  
**5TH FLOOR PLAN**  
 SCALE: 1/16" = 1'-0"




**CAMPUS CENTER**  
**6TH FLOOR PLAN**  
 SCALE: 1/16" = 1'-0"



Green Street Elevation



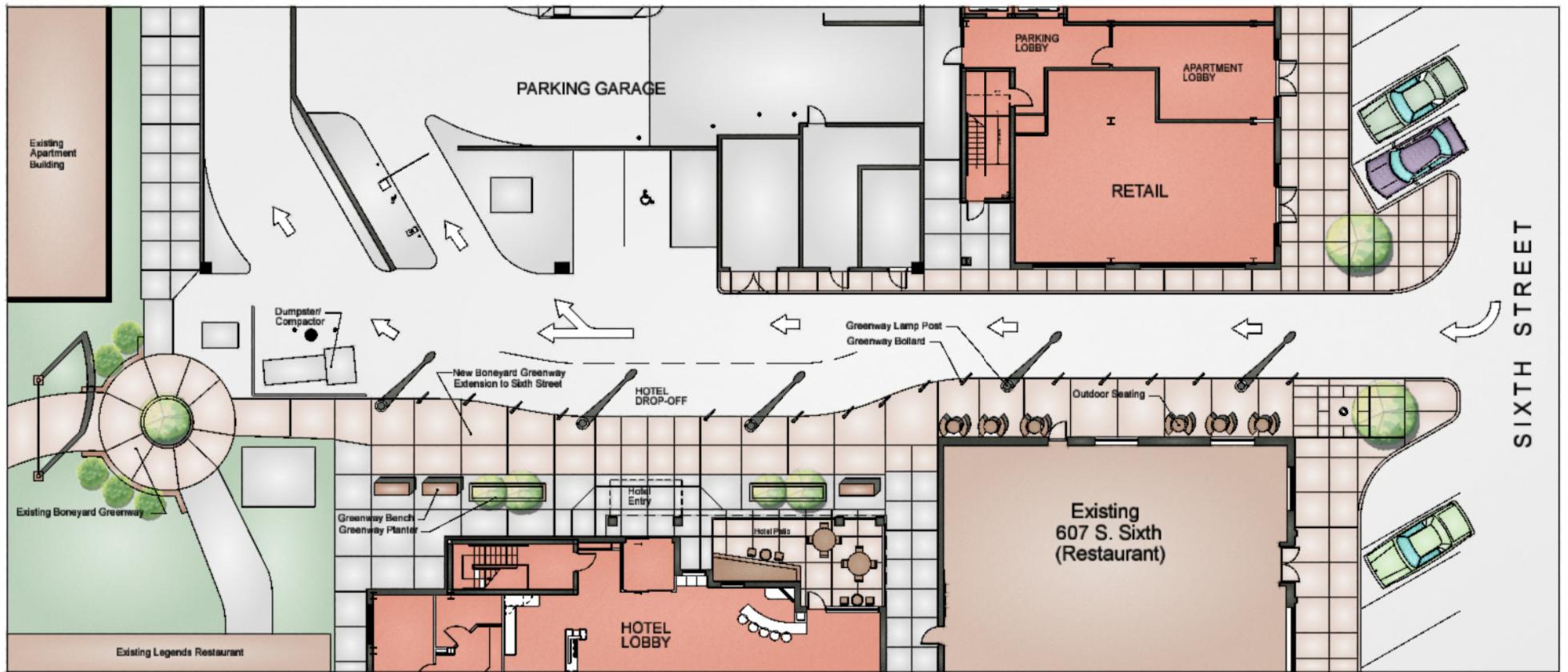
Sixth Street Elevation



Sixth Street Elevation



Healey Street Elevation



**Boneyard Greenway Extension - Enlarged Plan**

**Criteria for Approval**  
Major Amendment to the Preliminary / Final Planned  
Development Plan  
Campus Center (Parking Lot J),  
524 – 526 E. Green St. and 601 S. Sixth St.

1) **The proposal advances the purpose statements of the Planned Developments Article.**

The proposal advances the purpose statements of the Planned Development Article. The proposal:

- Allows flexibility;
- Encourages infill development;
- Promotes more efficient use of land because the property is located in an area that already contains public infrastructure and can be easily served by existing urban services such as police, fire, sewer, utilities, etc.;
- Incorporates site features so they become assets to the development by continuing the Boneyard Greenway;
- Provides additional amenities that would not otherwise be required under conventional zoning by providing 131 public parking spaces, and adding a new land use (hotel) to Campustown;
- Promote building styles and architectural styles that complement one another;
- Allow a mix of uses that are designed to negate potential conflicts that normally occur between incompatible land uses;
- Promote the most efficient arrangement of circulation systems, land use, and buildings;
- Promote environmentally sensitive developments by pursuing LEED Silver Certification; and
- Allow development, under a specifically approved design concept and site plan, which otherwise may not be permitted by the Zoning Ordinance.

2) **The proposal meets the minimum development requirements of the Zoning Ordinance except where waived.**

The development meets the minimum requirements of the Planned Development Article that apply to this development except where waived and described in #5 below.

The development meets the minimum requirements of the Planned Development Article that apply to this development, except for the following:

- Additional common open space requirements for major planned developments

**The proposal adequately and appropriately incorporates the design guidelines in Table VI-D that are appropriate to the type of development being proposed.**

The proposal incorporates the following design guidelines:

### *1.0 Site Plan Guidelines*

The design of the buildings provides a cohesiveness called for in this section. Trash and mechanical areas are screened, individual buildings are integrated into the overall site plan and sited in a coherent layout, addressing specific guidelines 1.1, 1.2, and 1.4.

### *2.0 Traffic Circulation Guidelines*

The circulation system provides for appropriate movement through and to adjacent sites. Adequate access for emergency and trash vehicles are provided, access points are minimized, a continuation of the Boneyard Greenway is provided, and covered bicycle parking is provided, addressing guidelines 2.1, 2.2, 2.3, and 2.5.

### *3.0 Architectural Quality Guidelines*

The brick, glass, and precast design is cohesive and compatible with buildings around the site. Multiple buildings relate to one another; the design creates visual interest through varying planes and materials; the use of natural materials is sensitive to the style and period of neighboring properties; building mass is broken up through setbacks and building offsets; commercial portions along Green Street and Sixth Street reflect the human scale through the use of storefronts; and buildings use finished building materials, addressing guidelines 3.1, 3.2, 3.3, 3.5, 3.6, and 3.9.

### *4.0 Environmental Guidelines*

The building is designed to save energy and will be certified at the LEED Silver level, meeting guideline 4.6.

### *5.0 Open Space Guidelines*

The proposal includes a continuation of the Boneyard Greenway, addressing guideline 5.5.

### *6.0 Privacy/Security Guidelines*

The development is proposed with views oriented to the exterior space, entrances are well-lit, public spaces and entrances are visible, and dwelling units are located on the second story and above to address guidelines 6.1, 6.2, 6.3, 6.4, and 6.5.

### *7.0 Landscaping and Screening Guidelines*

As an urban project, significant landscaping is not provided and screening is accomplished using building materials, but street trees will likely be provided along the Healey Street frontage as part of the City's Healey Street reconstruction project, and the Green Street and Sixth Street frontages will protect or repair streetscape to comply with the University Streetscape requirements and to address guideline 7.5.

3) **The proposal is consistent with the Comprehensive Plan and is in the best interest of the public.**

As the Comprehensive Plan was recently updated and included much public involvement and discussion, proposals that are consistent with the Comprehensive Plan would be in the best interest of the public. The proposal promotes the visions and guiding principles identified in the Comprehensive Plan in the following ways:

*Vision 1 - Growing City. Guiding Principle– Fiscally Sustainable Growth*

The proposal is located where existing service capacity and infrastructure already exists, providing for Fiscally Sustainable Growth.

*Vision 2 – Sustainable City. Guiding Principles – Reduce Consumption and Impact; Reduced Emissions; Development Patterns*

The development is located on an infill site, reducing the consumption of farmland on the edge of the community and is compact and contiguous to other development, thereby Reducing Consumption and Impact and supporting good Development Patterns.

*Vision 3 – Complete Neighborhoods. Guiding Principles – Well-Planned Mix of Uses; Well-Designed Density; Connectivity*

The proposal provides both commercial and residential uses, spreads the density among multiple buildings, and continues the Boneyard Greenway.

*Vision 4 – Community Identity. Guiding Principles – High Quality Urban Design; “Center City”*

The project is well designed and located within the Center City, with building offsets, varying roof heights, and a variety of materials.

*Vision 5 – Healthy Community. Guiding Principle – Walking Community; Alternative Transportation*

These principles are supported by buildings that are built to a human scale and face the public street and sidewalk system, covered bicycle parking, and a constructing a continuation of the Boneyard Greenway.

- 4) **Any proposed waivers or regulations or requirements of this Chapter meet the requirements of Section 37-327 of this Chapter, and any proposed waivers of Chapter 31 of this Code meet the requirements of Section 37-328 of this Chapter.**

The Plan Commission may recommend approval of and the Council may grant any requested waivers of regulations or requirements of this Chapter in a proposed Preliminary/Final Major Planned Development Plan provided that each of the following criteria has been satisfied:

*(a) The proposed waiver is in harmony with the general purpose and intent of this Chapter;*

Consistent with the general purpose of the Planned Development Chapter, the proposed waivers from Section 37-125.1 addressing the Campus Commercial Overlay design requirements, Section 37-191 requiring open space, Section 37-324 requiring dedication of a common open space, and Section 37-359 setting the parking requirement are associated with the redevelopment of an urban infill site with challenging site constraints, such as the site being bisected by both an underground stormwater conveyance system and an at-grade multi-use trail, and additional requirements associated with municipal goals for the development, such as the provision of public parking within the project. The development addresses a unique market niche with the addition of the hotel use and an emphasis on smaller bed count residential units. It provides an efficient land use of an otherwise underutilized site by replacing the surface parking lot with almost 110,000 square feet of retail, residential, and hotel use, plus a parking garage that contains public parking.

*(b) The proposed waiver will not alter the essential character of the locality;*

As the Campustown District is already characterized by a dense level of infill development with consistent and complementary uses, the proposed waivers will not alter the essential character of the locality. The proposed development will amplify and improve the essential character of the locality by constructing buildings at a site currently containing only a surface parking lot and that will complement adjacent buildings in terms of building design.

*(c) The proposed waiver will not impair an adequate supply of light and air to adjacent property, substantially increase the congestion in the streets, increase the danger of fire or crime, diminish the value of nearby property, nor impair the public health, safety, comfort, convenience, or general welfare.*

The proposed waivers will not impair an adequate supply of light and air to adjacent property as the waivers applicable to supplying light and air (Section 37-125.1, 37-191, and 37-324) are unrelated to areas of the development facing adjacent property. Setbacks of upper floors from adjacent property lines are often

well in excess of the proscribed minimum setbacks. Street congestion will not increase with the deliberate routing of traffic away from the overused Green Street corridor, and the waiver to reduce residential parking (Section 37-359) will result in fewer tenants with cars and therefore reduce the congestion in the area than if the required number of parking spaces were provided. Parking ingress and egress will not compete with already heavily used streets, as most of the parking traffic is directed toward Healey Street rather than Sixth and Green Streets. The development will include all code required fire prevention and life safety features to maintain the greatest possible protection against the danger of fire. The increased presence of people and activity throughout all times of the day will increase the sense of security for the public. The proposed development will enhance nearby property values with the added customer base and the improved options for public parking. Public health, safety, comfort, convenience, and general welfare will all benefit from the additional diversity of services and residential uses as well as from the indirect benefit of expanded property tax, hotel/motel tax, food and beverage tax, and sales tax revenue.

**Seven waivers from the Zoning Ordinance are requested:**

a. *Sec. 37-125.1(c)(4). Building facades shall have a minimum width of 90% of lot frontage.* Due to the garage entrances on Healey Street and the Boneyard Greenway connection to Sixth Street, those elevations do not meet the 90% minimum and driveway entrances exceed 22 feet.

*Justification:* The public parking provided and the continuation of the Boneyard Greenway, the multi-use trail that begins at Second Street, provide important benefits to the community and necessitate an adjustment to this requirement.

b. *Sec. 37-125.1(c)(5). Five foot setback of all floors above the third floor.* The Green Street façade sets back the requisite 5 feet at the second floor with the exception of two modules, each approximately 18' wide, that help define the linear character of the façade. These two modules maintain the grade level façade plane over the full height of the building. The grade level façade plane varies between 3 feet and 4 feet from the property line. One other exception to the set back is the main stair at the east end of the Green Street façade. Given the need to maintain a continuous stair enclosure over the full height of the building, no upper level setback is provided.

Due to the minimum site area necessary to accommodate a parking garage, the portion of the building on the northeast quadrant of the site has an atypically narrow footprint, limiting the ability to achieve a full 5 foot setback of upper levels from the lower building façade. Thus, the Sixth Street façade sets back 3'-4" beginning at the second floor with the exception of one module approximately 20' wide that help define the linear character of the façade and serves as a massing cue for the building entry location. This module maintains the grade level façade plane over the full height of the building. Beginning at the second floor line, the upper floors of the Healey Street façade set back approximately 3 feet from the lower building façade, but maintain a 6 foot setback from

the property line. One other exception to the set back is the main stair at the west end of the Healey Street façade. Given the need to maintain a continuous stair enclosure over the full height of the building, no upper level setback is provided. Given the structural and functional restrictions of the parking deck, no additional setback is anticipated for upper levels of the parking deck's Healey Street façade.

*Justification:* The majority of the building facades meet the requirement. The exceptions help to provide more articulation in the building façade and therefore a more interesting and detailed elevation.

c. *Sec. 37-125.1(c)(6). Transparency at the ground floor.* Project does not comply with the 75% minimum transparency requirement in the following locations: the parking deck and building service space of the narrow Healey Street frontage for which this requirement would not be reasonably applicable, and the façade along Green Street, which provides for 74% transparency to provide a base to the building that is architecturally in scale with the remainder of the building. Stair towers are also excluded from the frontage calculation.

*Justification:* A required component of the project, the public parking needs to be easily accessed and viewed from the public right-of-way to encourage its use. In terms of the Green Street façade, the brick piers on the ground level are in scale with the remainder of the building, resulting in a minimal reduction in the level of transparency at the ground level. This reduction allows for better architectural design to provide a solid base to the rest of the building. Emergency egress stair towers are a reasonable exception to the transparency requirement and are provided only where required by building code.

d. *Sec. 37-125.1(c)(7). Transparency above the ground floor.* Project does not comply with the 30% minimum transparency requirement for the Healey Street residential portions of the building (at 24%) to remain consistent with the punched window motif of the architectural style while maximizing the amount of transparency. Additional non-compliant facades are those facing a street but not actually fronting on the street, such as the eastern façade of the Green Street building and the southern façade of the Sixth Street building. Excluding the elevations not fronting on a street is reasonable for this project given (i) there is an existing two story building between the referenced elevations and the street, largely screening the elevations from view; and (ii) the referenced elevations set back from the property line between 70 feet to 100 feet, mitigating their perceptual impact on the overall façade. Stair towers are also excluded from the area calculation.

*Justification:* A portion of the Healey Street façade is the side of the narrow side of the building that fronts along Sixth Street, providing a limited area to provide windows. The other portion of the Healey Street façade, over the parking structure, provides the maximum transparency while maintaining a consistent architectural design with the remainder of the project. The other elevations in question “face” the street and therefore trigger the transparency requirement, but their setbacks of at least 70 feet results in the inability for a pedestrian at the ground level to be negatively impacted by a lack of windows. In addition, the developer has future plans to redevelop one on the adjacent sites, which would screen most of these facades in the future.

e. *Sec. 37-125.1(c)(12). Parking deck screening.* The Healey Street facing façade of the parking deck will provide partial screening at guardrail height. This height is tall enough to screen vehicle headlights from projecting out of the deck. The balance of the wall will be open to provide the building code required ventilation necessary for an open parking structure.

*Justification:* The provision of public parking offsets the screening requirement, as the public parking will be more easily located. The proposed construction materials for the walls are a patterned precast concrete panel system similar to the one used for the City-owned public parking garage in downtown.

f. *Section 37-324. Common Open Space Requirements for Major Planned Developments.* This section requires that a parcel designated as common open space of at least 6,000 square feet is conveyed to a not-for-profit corporation, guaranteed by a restrictive covenant, or dedicated to the Champaign Park District. This requirement is partially met by continuing the Boneyard Greenway and providing public access to both the Greenway and the adjacent courtyard, however the width of the proposed Greenway (10 feet) does not meet the minimum dimension for open space of 18 feet. Other common areas are dispersed throughout the development, including an interior pool and fitness area and four courtyards on the second and fifth floors.

*Justification:* Providing ground level open space in an urban environment is contrary to the goals of providing dense development that continues the building wall. The requirement to dedicate open space is partially met by this project by continuing the Boneyard Greenway and providing public access to both the Greenway and the adjacent courtyard, however the width of the proposed Greenway (10 feet) does not meet the minimum dimension for open space of 18 feet and therefore cannot be fully counted in the calculation, despite its full functionality as open space.

g. *Sec. 37-359. Required Number of parking Spaces by Use (specifically Sec. 37-359.2 Residential and Related Uses).* A reduction of 26 parking spaces is requested for the project. The zoning requirement for parking is 71 spaces for the 88 residential units. Neither the retail nor the hotel uses have a parking requirement due to their location within the Central Business District. While the project will provide a total of 176 parking spaces, 131 of those spaces are required to be accessible to the public through the Development Agreement. Therefore, as a practical matter, a total of 202 parking spaces are “required” for the project (71 residential spaces + 131 public spaces), leading to a request for a waiver of 26 parking spaces.

*Justification:* The provision of 131 public parking spaces is ample justification to reduce the parking requirement by 26 spaces.

The above requested waivers are in harmony with the general intent of the Planned Development Chapter and do not alter the essential character of the locality. The waivers will also not impair an adequate supply of light and air to adjacent property or substantially increase the congestion in the streets. In Staff’s opinion the waivers will not diminish the value of nearby property, nor impair the public health, safety, comfort, convenience, or general welfare.